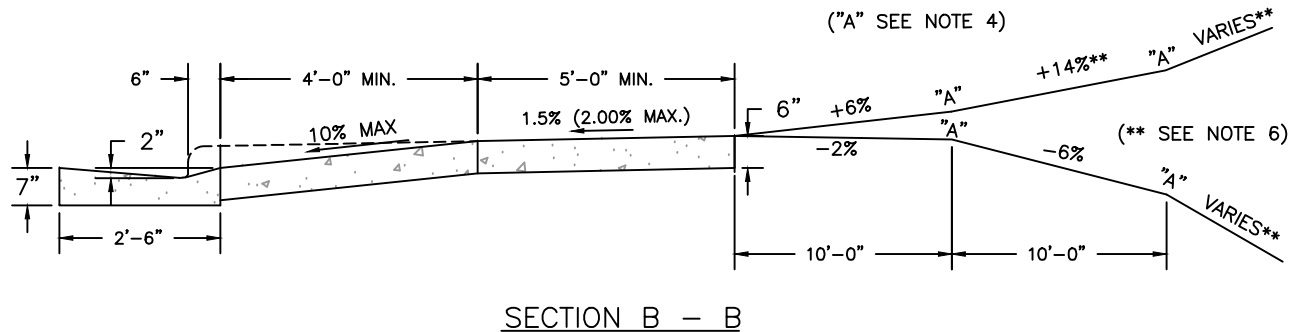
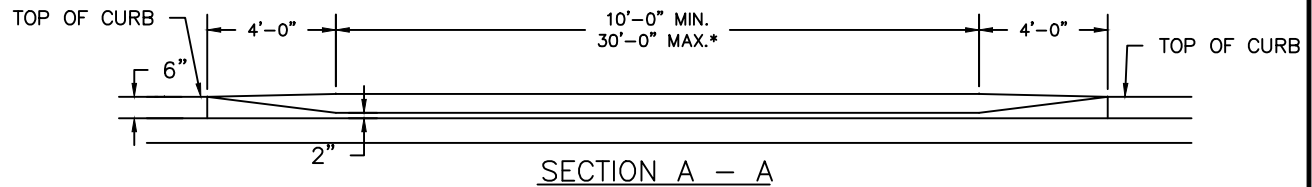
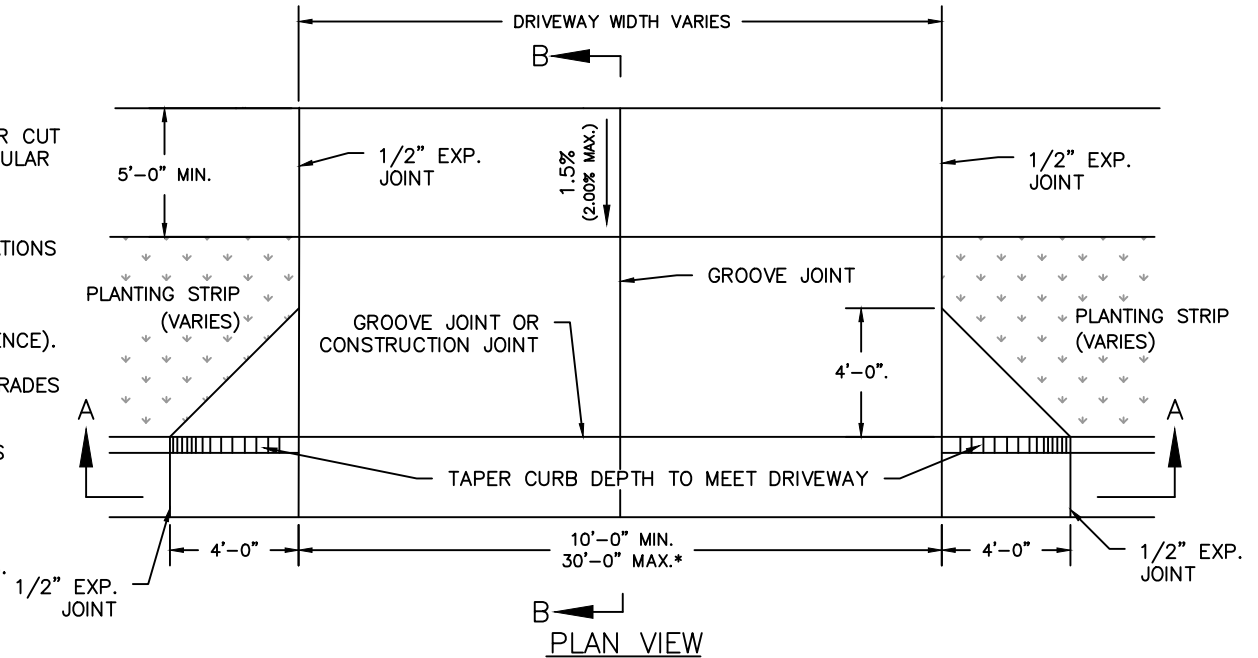


NOTES:

1. ALL CONCRETE TO BE 3500 P.S.I.
2. ALL CURB OR CURB AND GUTTER AND SIDEWALKS ARE TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAWCUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE STD. NO. 1012 FOR JOINT DETAIL.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. "A" BREAKOVER SHALL BE 8% OR LESS (A = ALGEBRAIC DIFFERENCE).
5. PRIOR APPROVAL IS REQUIRED BY LEXINGTON ENGINEERING ON GRADES EXCEEDING WHAT ARE SHOWN.
6. ** PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
7. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
8. CONCRETE SIDEWALK WITHIN DRIVEWAY SHALL HAVE 6" THICKNESS.



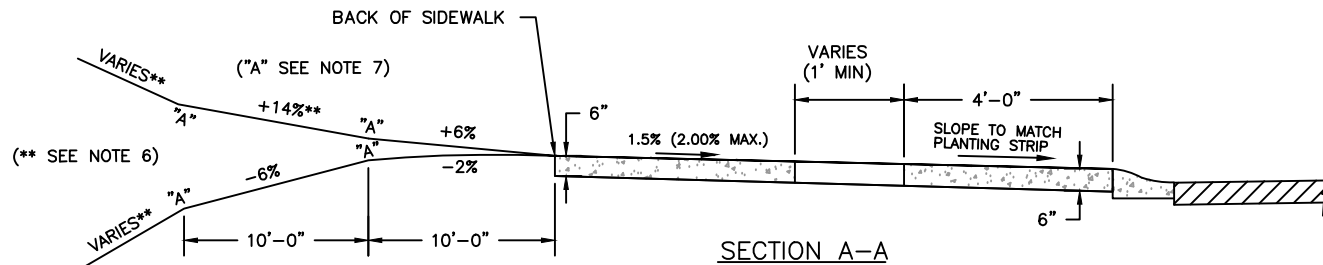
NOT TO SCALE

RESIDENTIAL DROP CURB DRIVEWAY WITH PLANTING STRIP (2'-6" CURB AND GUTTER)

REV.	STD. NO.
1	1201

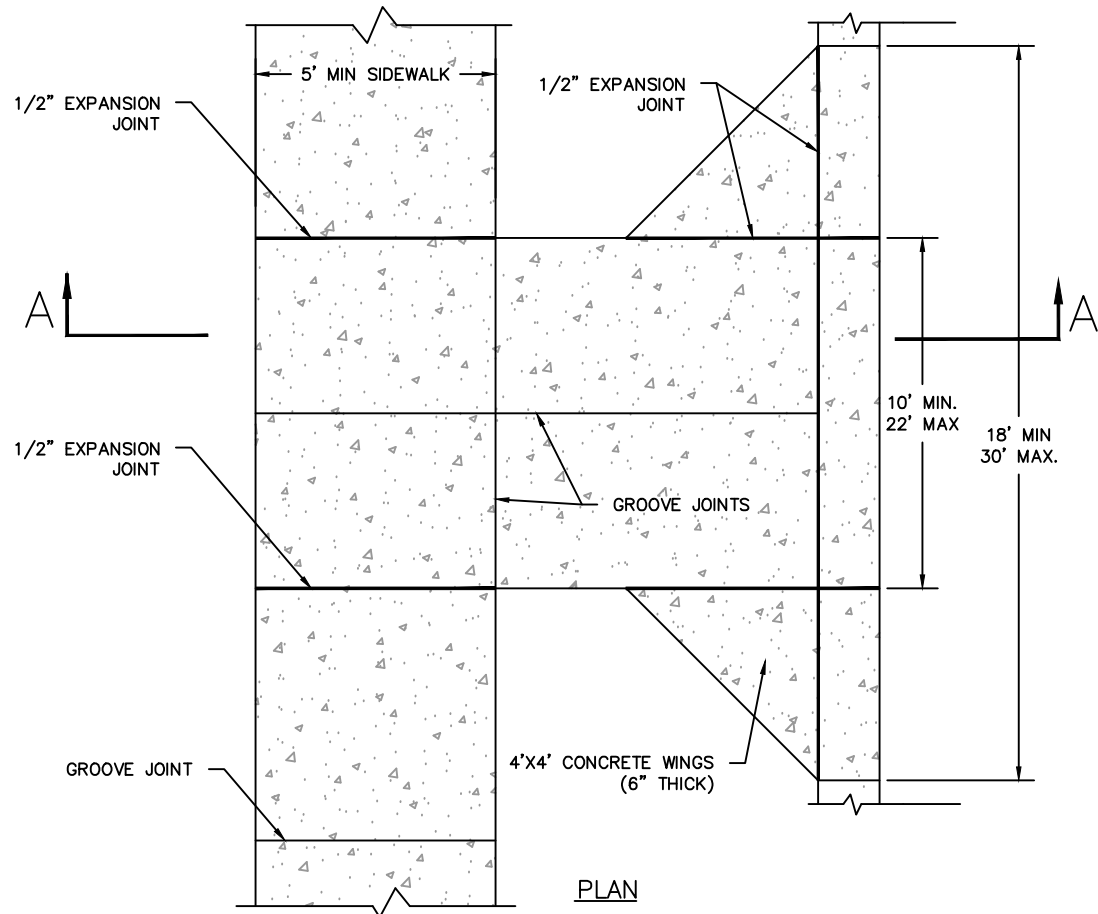


CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS



NOTES:

1. THE ELEVATION OF THE SIDEWALK SHALL NOT BE LESS THAN SIX INCHES OR MORE THAN EIGHTEEN INCHES ABOVE THE ROADWAY CROWN. THIS ELEVATION DIFFERENTIAL SHALL BE CONSISTENT WITHIN EACH BLOCK.
2. ALL CONCRETE TO BE 3500 PSI STRENGTH.
3. ALL CONSTRUCTION PRACTICES, INCLUDING COMPACTION, CURING, FINISHING, ETC. SHALL BE IN ACCORDANCE WITH THE LEXINGTON INFRASTRUCTURE DEVELOPMENT STANDARDS.
4. PLANTING STRIP SHALL BE GRADED WITH A CROSS SLOPE BETWEEN 1/4 IN. PER FOOT AND 1 1/4 IN. PER FOOT EXCEPT WHERE EXCESSIVE NATURAL GRADES MAKE THIS REQUIREMENT IMPRACTICAL. IN SUCH CASES, THE CITY ENGINEER MAY AUTHORIZE A SUITABLE GRADE.
5. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS, INCLUDING BUT NOT LIMITED TO SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
6. **PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
7. "A" BRAKOVER SHALL BE 8% OR LESS (A = ALGEBRAIC DIFFERENCE).
8. PRIOR APPROVAL IS REQUIRED BY LEXINGTON ENGINEERING ON GRADES EXCEEDING WHAT ARE SHOWN.
9. CONCRETE SIDEWALK SHALL BE 6" THICK WITHIN DRIVEWAY.



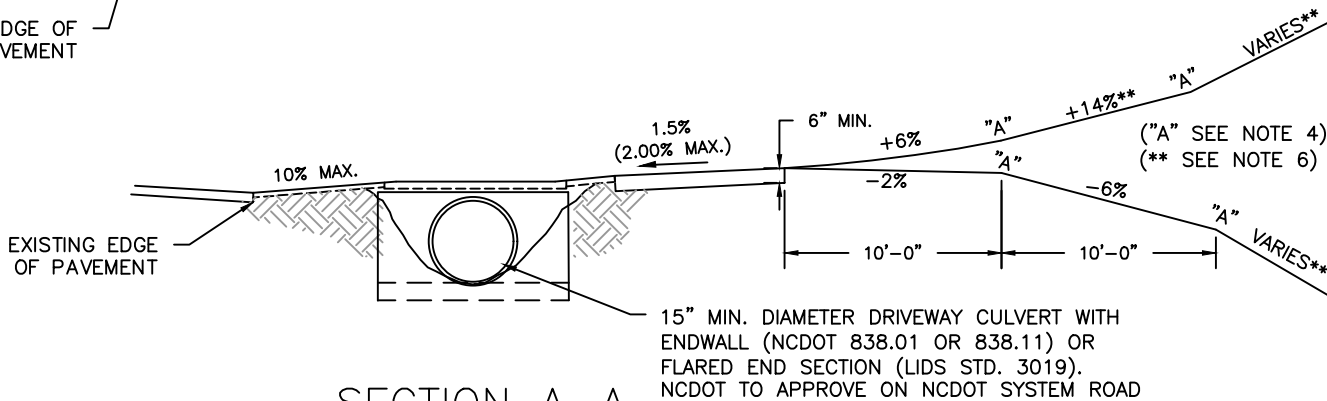
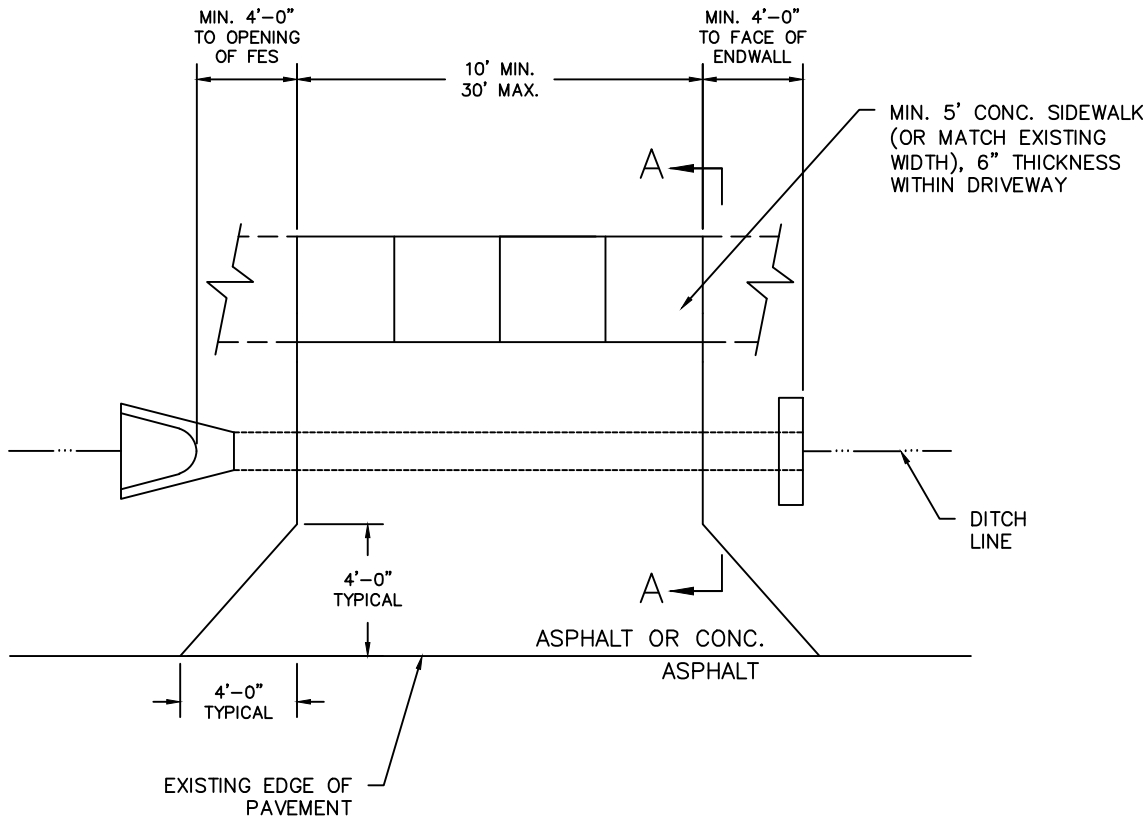
NOT TO SCALE

**RESIDENTIAL DRIVEWAY
(2'-0" VALLEY CURB)**

REV.	STD. NO.
1	1202



**CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS**



SECTION A-A

NOT TO SCALE

NOTES:

1. TO BE USED ON ROADS WITHOUT CURB AND GUTTER AND WHERE CURB AND GUTTER IS NOT BEING INSTALLED. (MUST MEET BOTH CRITERIA)
2. ALL CONCRETE TO BE 3500 P.S.I. COMPRESSIVE STRENGTH.
3. "A" BREAKOVER SHALL BE 8% OR LESS.
4. ** PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
5. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
6. PRIOR APPROVAL IS REQUIRED BY LEXINGTON ENGINEERING ON GRADES EXCEEDING WHAT ARE SHOWN.
7. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
8. DRIVEWAY CULVERT SHALL BE MIN. 15" RCP UNLESS OTHERWISE APPROVED BY LEXINGTON ENGINEERING (LARGER PIPE SIZES MAY BE REQUIRED DEPENDANT UPON FLOW)
9. PIPES MAY NOT BE SMALLER THAN UPSTREAM PROPERTIES.
10. CONCRETE SHALL BE FLUSH WITH EXISTING PAVEMENT ON EXISTING STREETS.

REV.	STD. NO.
1	1203A

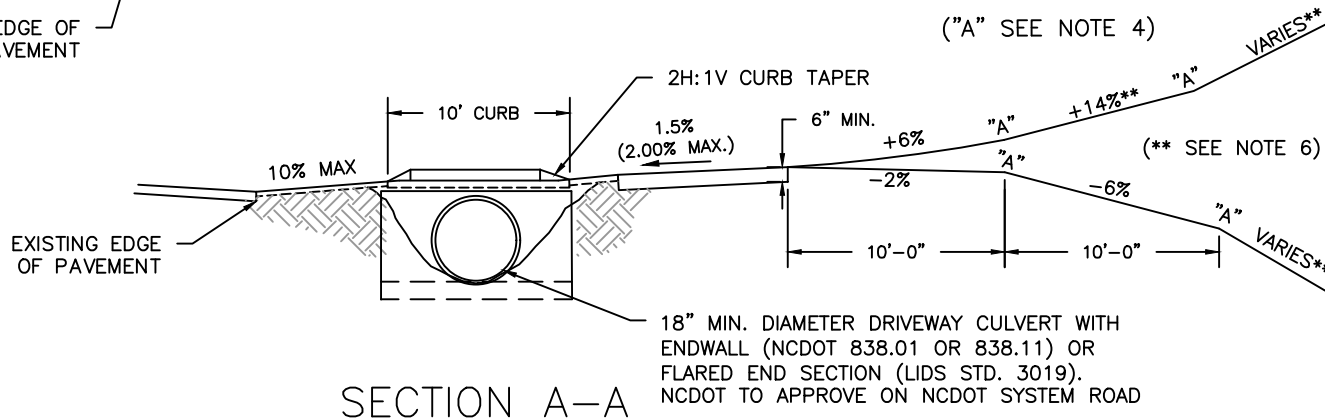
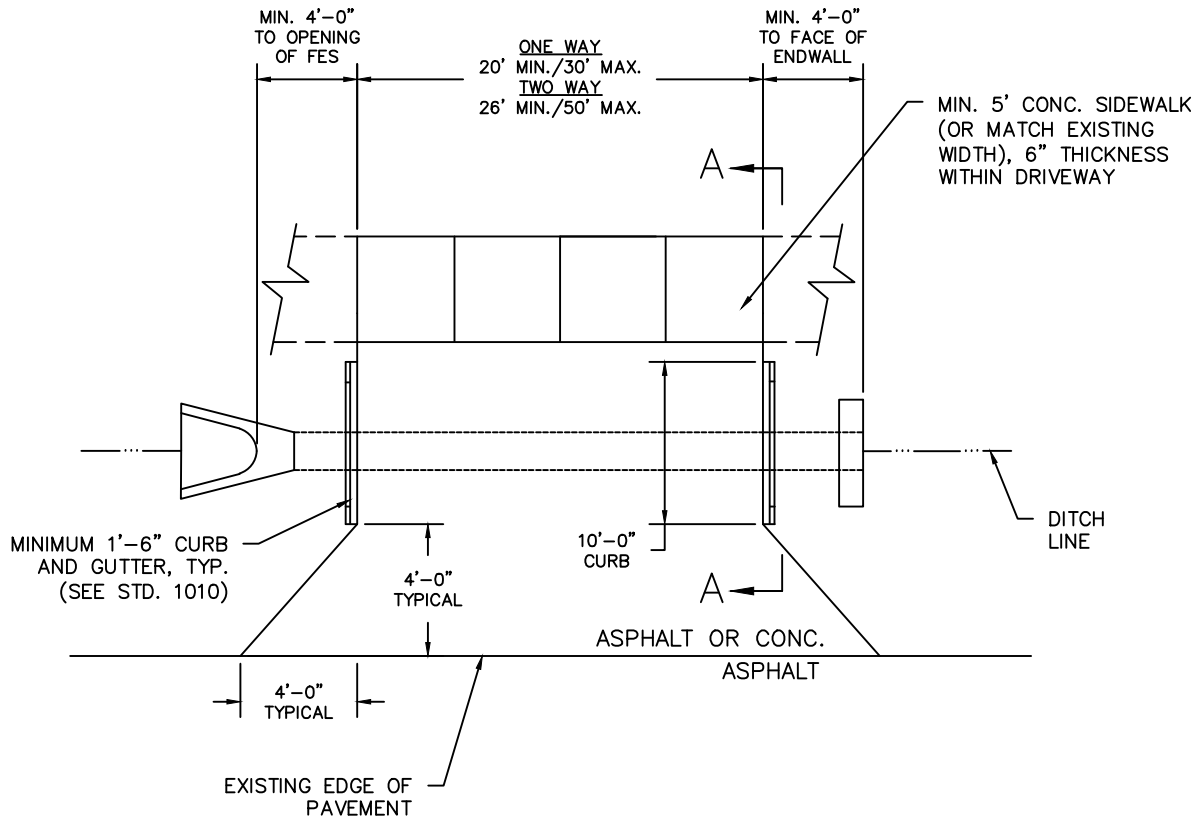
RESIDENTIAL DITCH-TYPE DRIVEWAY



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS

NOTES:

1. TO BE USED ON ROADS WITHOUT CURB AND GUTTER AND WHERE CURB AND GUTTER IS NOT BEING INSTALLED. (MUST MEET BOTH CRITERIA)
2. ALL CONCRETE TO BE 3000 P.S.I. COMPRESSIVE STRENGTH.
3. THIS STANDARD IS TYPICALLY FOR COMMERCIAL APPLICATION. FOR RESIDENTIAL DRIVEWAY CONSTRUCTION, USE AT THE DISCRETION OF THE CITY.
4. "A" BREAKOVER SHALL BE 8% OR LESS.
5. ** PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
6. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
7. PRIOR APPROVAL IS REQUIRED BY LEXINGTON ENGINEERING ON GRADES EXCEEDING WHAT ARE SHOWN.
8. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
9. DRIVEWAY CULVERT SHALL BE MIN. 15" RCP UNLESS OTHERWISE APPROVED BY LEXINGTON ENGINEERING (LARGER PIPE SIZES MAY BE REQUIRED DEPENDANT UPON FLOW)
10. PIPES MAY NOT BE SMALLER THAN UPSTREAM PROPERTIES.
11. CONCRETE SHALL BE FLUSH WITH EXISTING PAVEMENT ON EXISTING STREETS.



SECTION A-A

NOT TO SCALE

COMMERCIAL DITCH-TYPE DRIVEWAY

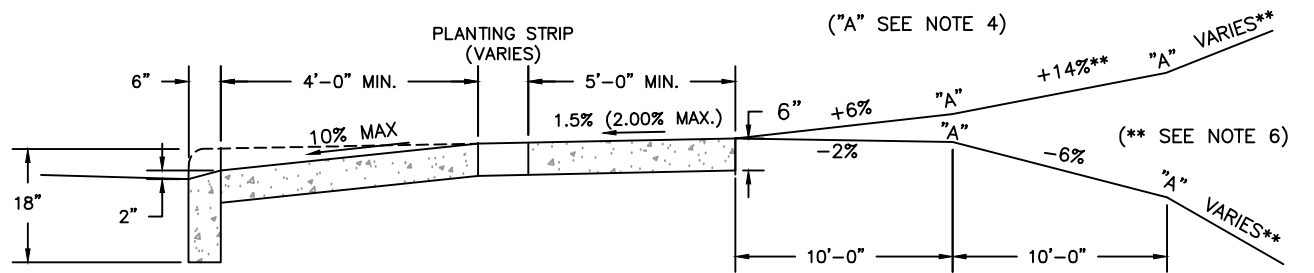
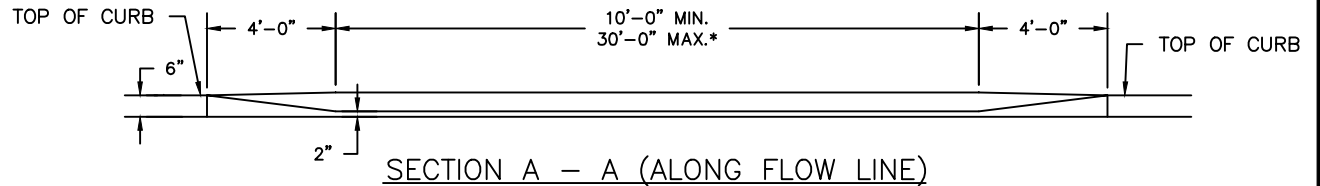
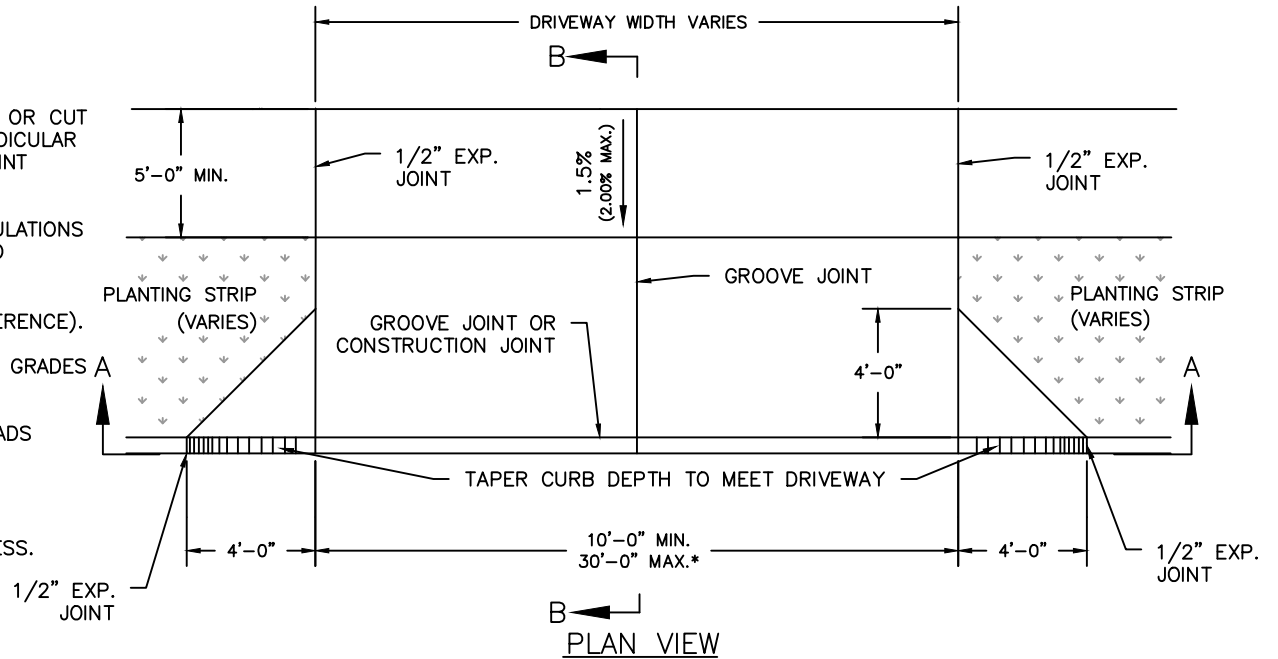
REV.	STD. NO.
1	1203B



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS

NOTES:

1. ALL CONCRETE TO BE 3500 P.S.I.
2. ALL CURB OR CURB AND GUTTER AND SIDEWALKS ARE TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAWCUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE STD. NO. 1012 FOR JOINT DETAIL.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. "A" BREAKOVER SHALL BE 8% OR LESS (A = ALGEBRAIC DIFFERENCE).
5. PRIOR APPROVAL IS REQUIRED BY LEXINGTON ENGINEERING ON GRADES EXCEEDING WHAT ARE SHOWN.
6. ** PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
7. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
8. CONCRETE SIDEWALK WITHIN DRIVEWAY SHALL HAVE 6" THICKNESS.



NOT TO SCALE

RESIDENTIAL DROP CURB DRIVEWAY WITH PLANTING STRIP (18" VERTICAL CURB)

REV.	STD. NO.
1	1204



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS

NOTES:

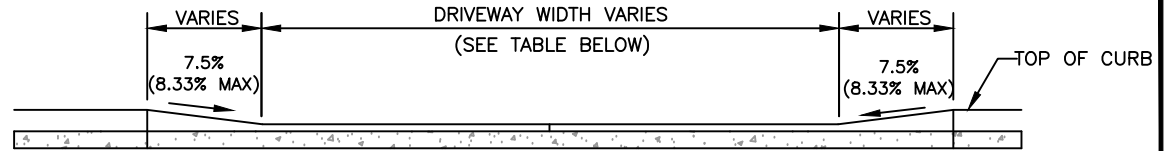
1. 1/2" EXPANSION JOINTS REQUIRE INSTALLATION OF ONE 1/2" THICK PIECE OF BITUMINOUS FIBER THROUGH THE ENTIRE SLAB. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
2. TO LIMIT STORM WATER FLOW DOWN DRIVEWAYS, USE STANDARD 1205C FOR DRIVEWAYS NEAR LOW POINTS.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. "A" BREAKOVER SHALL BE 8% OR LESS (A = ALGEBRAIC DIFFERENCE).
5. PRIOR APPROVAL IS REQUIRED BY CDOT ON GRADES EXCEEDING WHAT ARE SHOWN.
6. ** PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.

GENERAL NOTES:

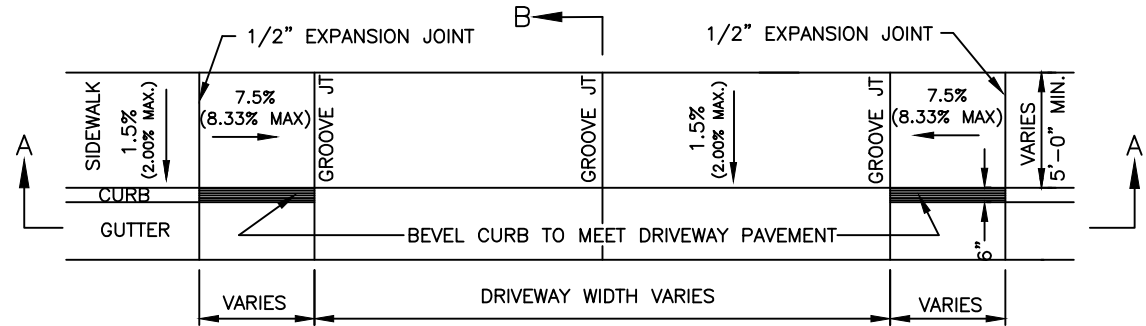
- ALL CONCRETE TO BE 3500 P.S.I. COMPRESSIVE STRENGTH.
- ALL CURB, CURB AND GUTTER AND SIDEWALKS ARE TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED.
- SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT.
- SEE STD. NO 1012 FOR DETAIL OF EXPANSION JOINT AND GROOVE JOINT.

DRIVEWAY WIDTH		
DRIVEWAY TYPE	MINIMUM	MAXIMUM
RESIDENTIAL: LOCAL/COLLECTOR THOROUGHFARE *	10'	30'
ONE-WAY COMMERCIAL	20'	30'
TWO-WAY COMMERCIAL	26'	50'

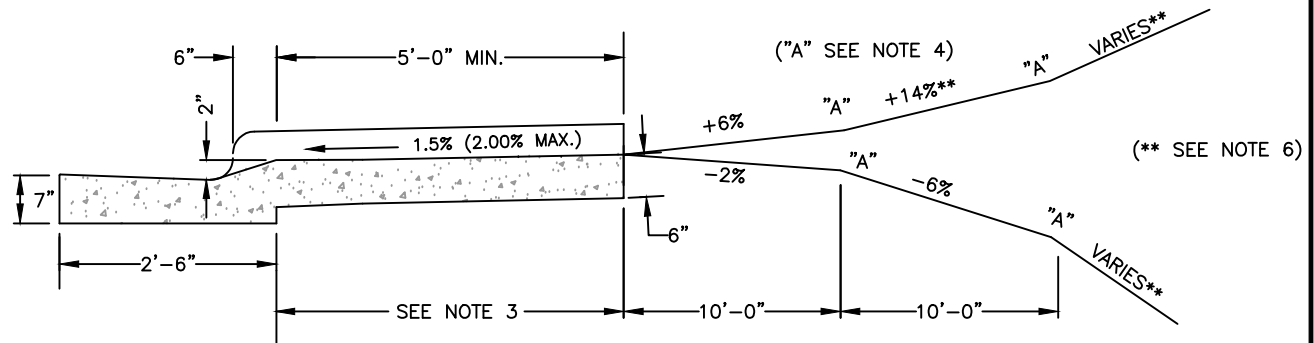
* MUST PROVIDE ON-SITE TURNAROUND



SECTION A - A



PLAN



SECTION B - B

NOT TO SCALE

RESIDENTIAL & COMMERCIAL DROP CURB DRIVEWAY WITH SIDEWALK ABUTTING CURB

(2'-6" CURB AND GUTTER)

REV.	STD. NO.
1	1205A



**CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS**

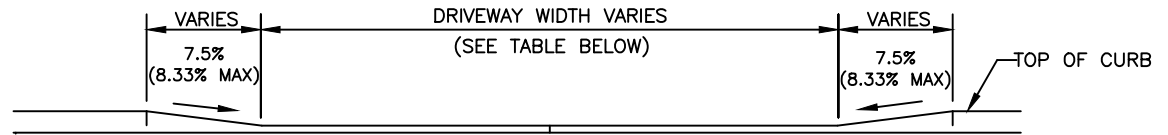
NOTE:

- 1/2" EXPANSION JOINTS REQUIRE INSTALLATION OF ONE 1/2" THICK PIECE OF BITUMINOUS FIBER THROUGH THE ENTIRE SLAB. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
- TO LIMIT STORM WATER FLOW DOWN DRIVEWAYS, USE STANDARD 1205C FOR DRIVEWAYS NEAR LOW POINTS.
- ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
- "A" BRAKEOVER SHALL BE 8% OR LESS (A = ALGEBRAIC DIFFERENCE).
- PRIOR APPROVAL IS REQUIRED BY LEXINGTON ENGINEERING ON GRADES EXCEEDING WHAT ARE SHOWN.
- ** PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.

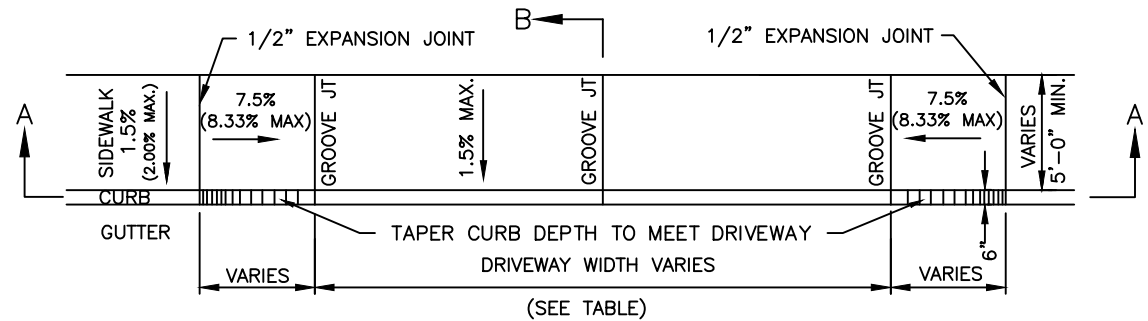
GENERAL NOTES:

- ALL CONCRETE TO BE 3500 P.S.I. COMPRESSIVE STRENGTH.
- ALL CURB, CURB AND GUTTER AND SIDEWALKS ARE TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED.
- SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT.
- SEE STD. NO 1012 FOR DETAIL OF EXPANSION JOINT AND GROOVE JOINT.

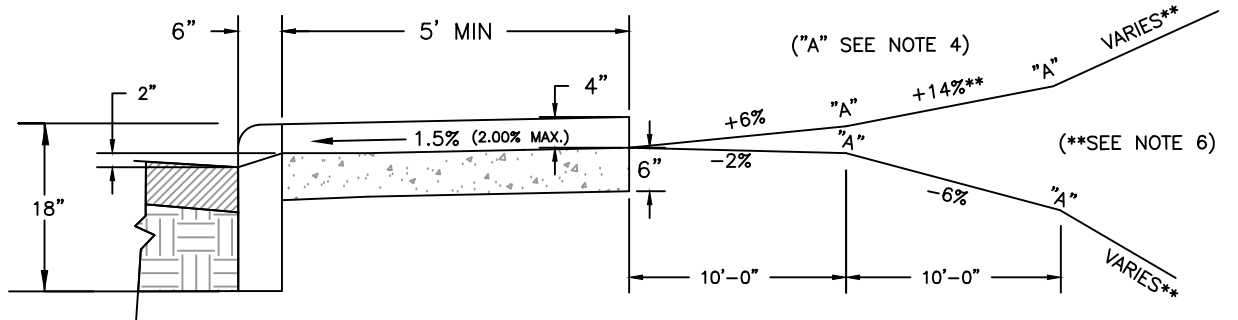
DRIVEWAY WIDTH		
TYPE DRIVEWAY	MINIMUM	MAXIMUM
RESIDENTIAL: LOCAL/COLLECTOR	10'	30'
ONE-WAY COMMERCIAL	20'	30'
TWO-WAY COMMERCIAL	26'	50'



SECTION A - A



PLAN



SECTION B - B

NOT TO SCALE

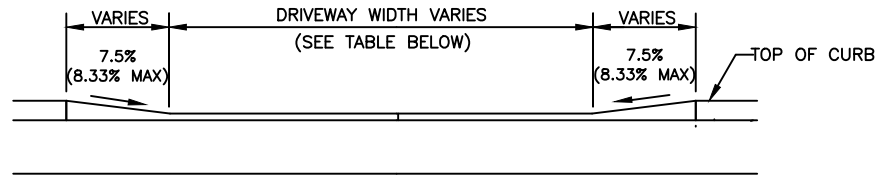
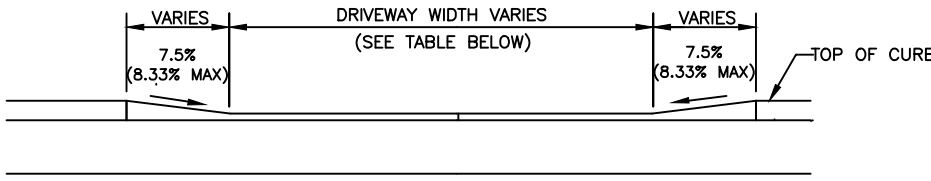
RESIDENTIAL & COMMERCIAL DROP CURB DRIVEWAY WITH SIDEWALK ABUTTING CURB

(18" VERTICAL CURB)

REV.	STD. NO.
1	1205B

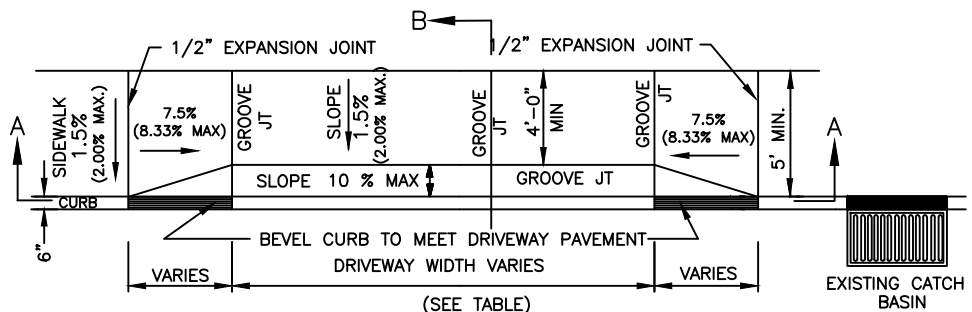
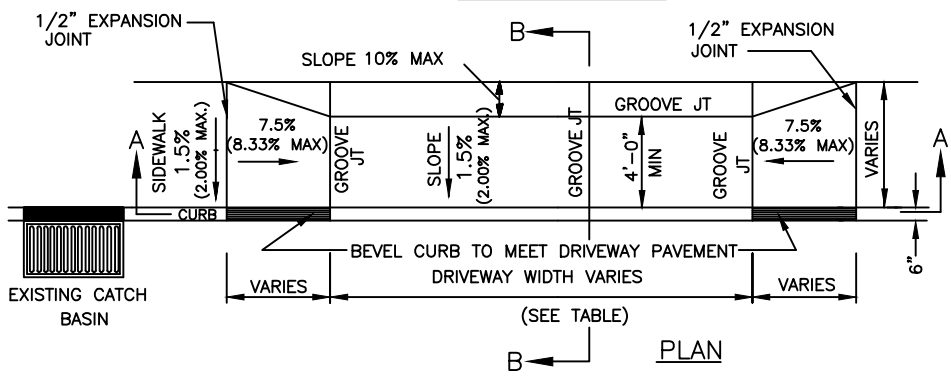


**CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS**



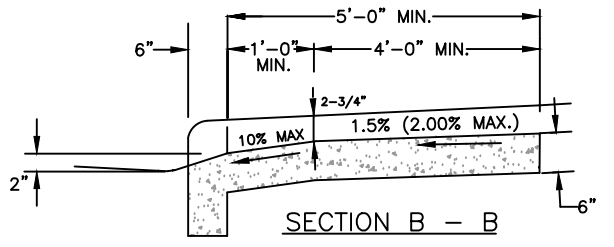
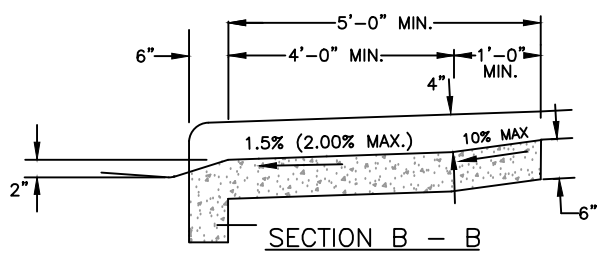
SECTION A - A

SECTION A - A



PLAN

PLAN



SECTION B - B

SECTION B - B

DRIVEWAY WIDTH		
DRIVEWAY TYPE	MINIMUM	MAXIMUM
RESIDENTIAL: LOCAL/COLLECTOR THOROUGHFARE*	10'	30'
ONE-WAY COMMERCIAL	20'	30'
TWO-WAY COMMERCIAL	26'	50'

* MUST PROVIDE ON-SITE TURNAROUND

NOTES

1. USED AT LOW POINTS IN ROADWAYS WITH 2'-6" CURB AND GUTTER OR 6" X 18" CURB AS DIRECTED BY CITY ENGINEER.
2. SEE STANDARDS 1205A & 1205B FOR ADDITIONAL DETAILS.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.

NOT TO SCALE

DROP CURB DRIVEWAY WITH SIDEWALK
ABUTTING CURB AT LOW-POINT

(18" VERTICAL CURB)

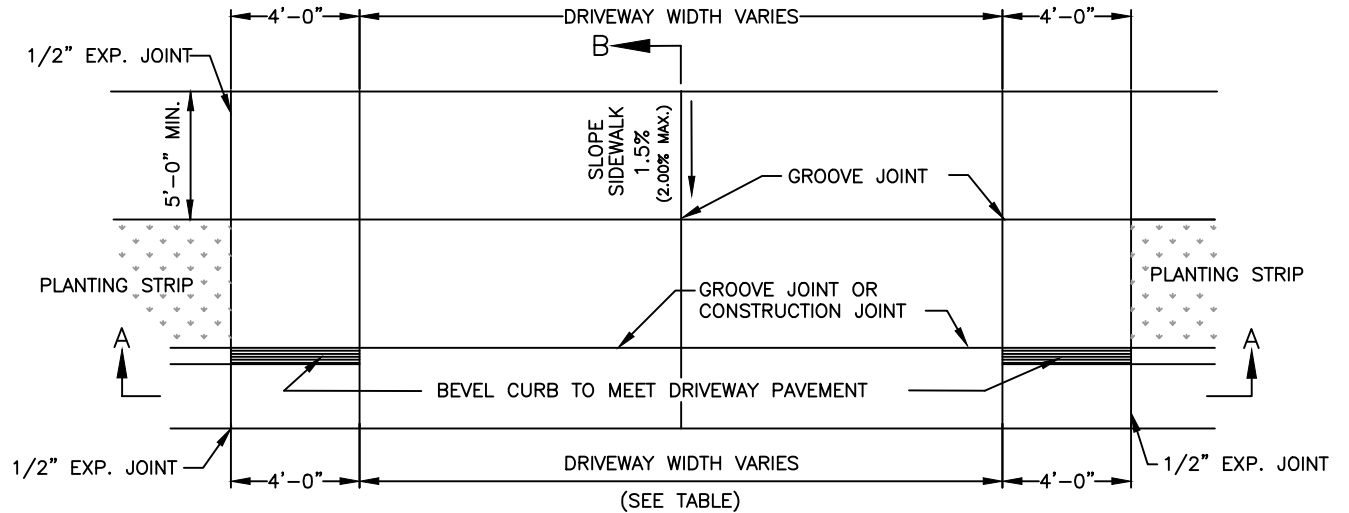
REV.	STD. NO.
1	1205C



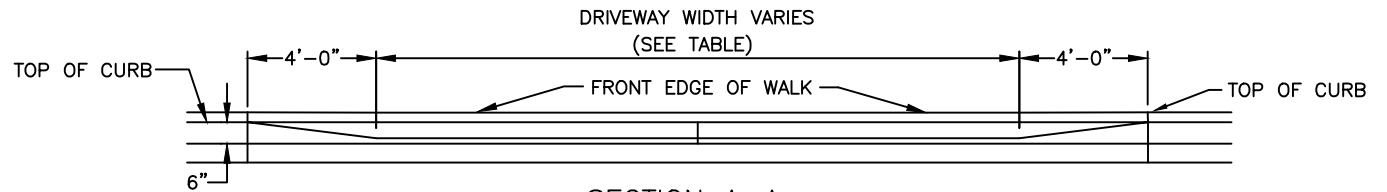
CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS

NOTES:

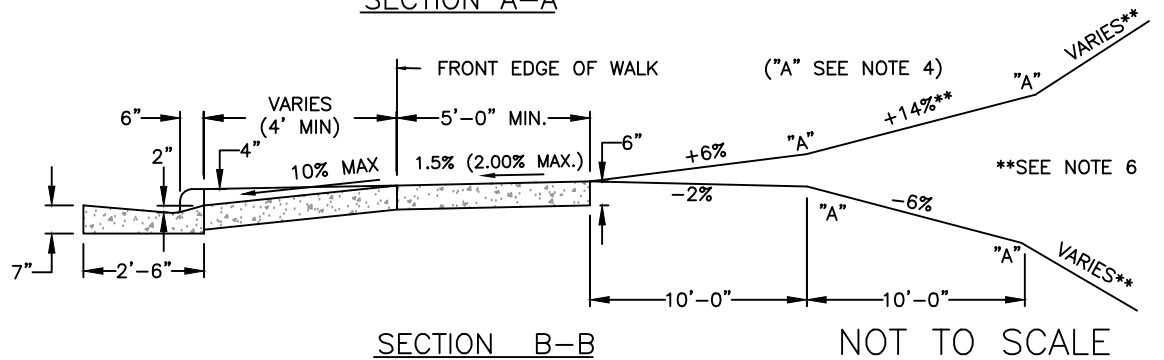
1. ALL CONCRETE TO BE 3500 P.S.I. COMPRESSIVE STRENGTH.
2. AT ALL DRIVEWAYS, SIDEWALKS TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE STD. NO. 1012 FOR JOINT DETAIL.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. "A" BREAKOVER SHALL BE 8% OR LESS (A=ALGEBRAIC DIFFERENCE).
5. PRIOR APPROVAL IS REQUIRED BY CDOT ON GRADES EXCEEDING THE GRADES SHOWN ON THIS DETAIL.
6. **PER NC IFC SECTION D103.2, FIRE APPARATUS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
7. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
8. THIS DETAIL IS ONLY FOR USE WHEN PLANTING STRIP IS 6' OR LESS IN WIDTH. USE LIDS STD. 1209 WITH LARGER PLANTING STRIP.



PLAN VIEW



SECTION A-A



SECTION B-B

NOT TO SCALE

DRIVEWAYS CLASSIFICATION		
TYPE DRIVEWAYS	MINIMUM	MAXIMUM
ONE-WAY COMMERCIAL	20'	30'
TWO-WAY COMMERCIAL	26'	50'*

* NEED MORE THAN ONE CONTRACTION JOINT IN CENTER.

COMMERCIAL DROP CURB DRIVEWAY WITH PLANTING STRIP TYPE 1

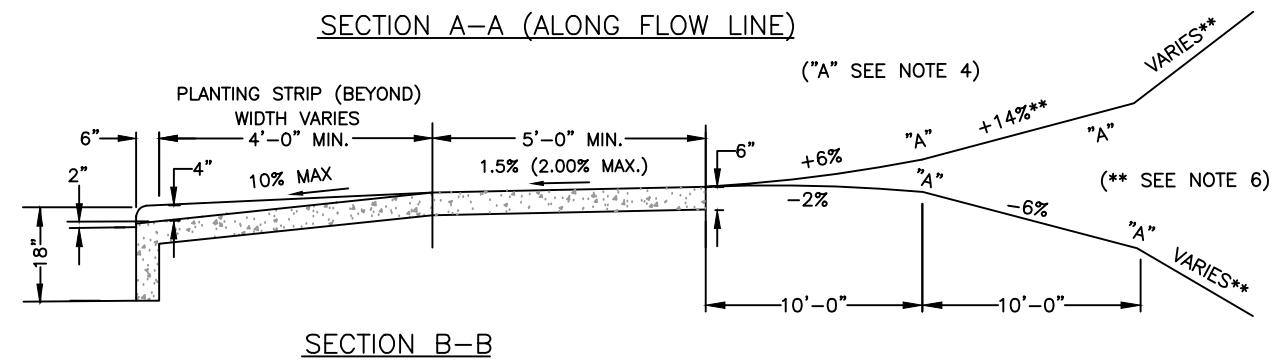
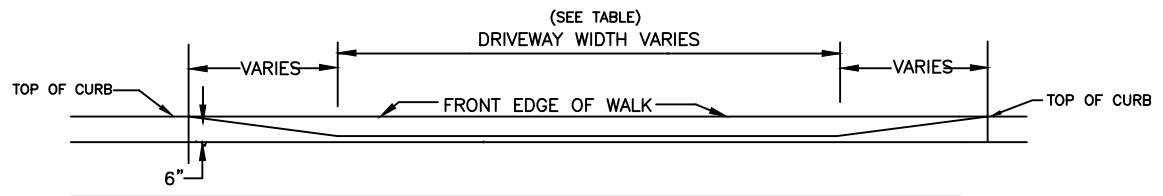
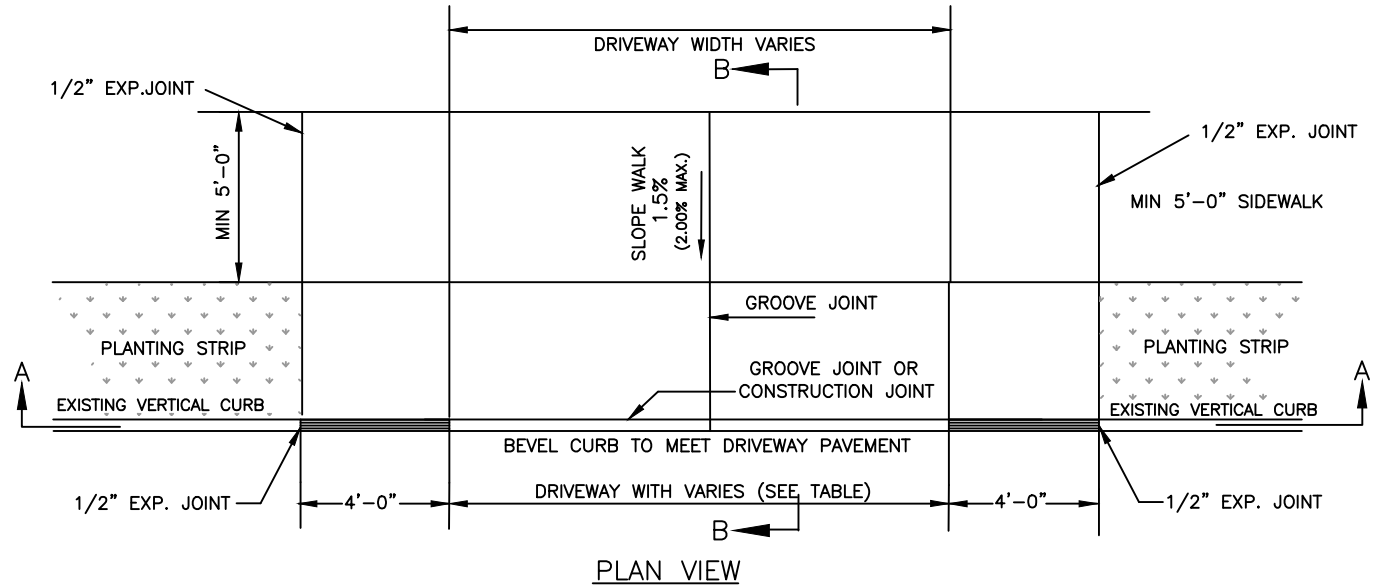
REV.	STD. NO.
1	1206



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS

NOTES:

1. ALL CONCRETE TO BE 3500 P.S.I.
2. ALL CURB OR CURB AND GUTTER AND SIDEWALK ARE TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE STD. NO. 1012 FOR JOINT DETAIL.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. "A" BREAKOVER SHALL BE 8% OR LESS. (A=ALGEBRAIC DIFFERENCE)
5. PRIOR APPROVAL IS REQUIRED BY CDOT FOR GRADES EXCEEDING THE GRADES SHOWN ON THIS DETAIL
6. **PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
7. JOINT MATERIAL SHOULD BE FLUSH WITH CONCRETE.
8. THIS DETAIL IS ONLY FOR USE WHEN PLANTING STRIP IS 6' OR LESS IN WIDTH. USE DRIVEWAY STD. 1209 WITH LARGER PLANTING STRIP.



DRIVEWAYS CLASSIFICATION		
TYPE DRIVEWAYS	MINIMUM	MAXIMUM
ONE-WAY COMMERCIAL	20'	30'
TWO-WAY COMMERCIAL	26'	50'*

* NEED MORE THAN ONE CONTRACTION JOINT IN CENTER

NOT TO SCALE

COMMERCIAL DROP CURB DRIVEWAY WITH PLANTING STRIP TYPE 2

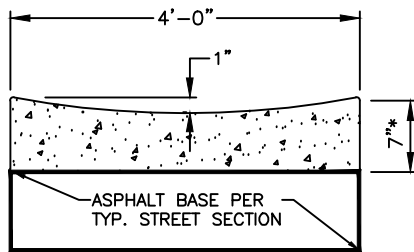
REV.	STD. NO.
1	1207



CITY OF LEXINGTON INFRASTRUCTURE DEVELOPMENT STANDARDS

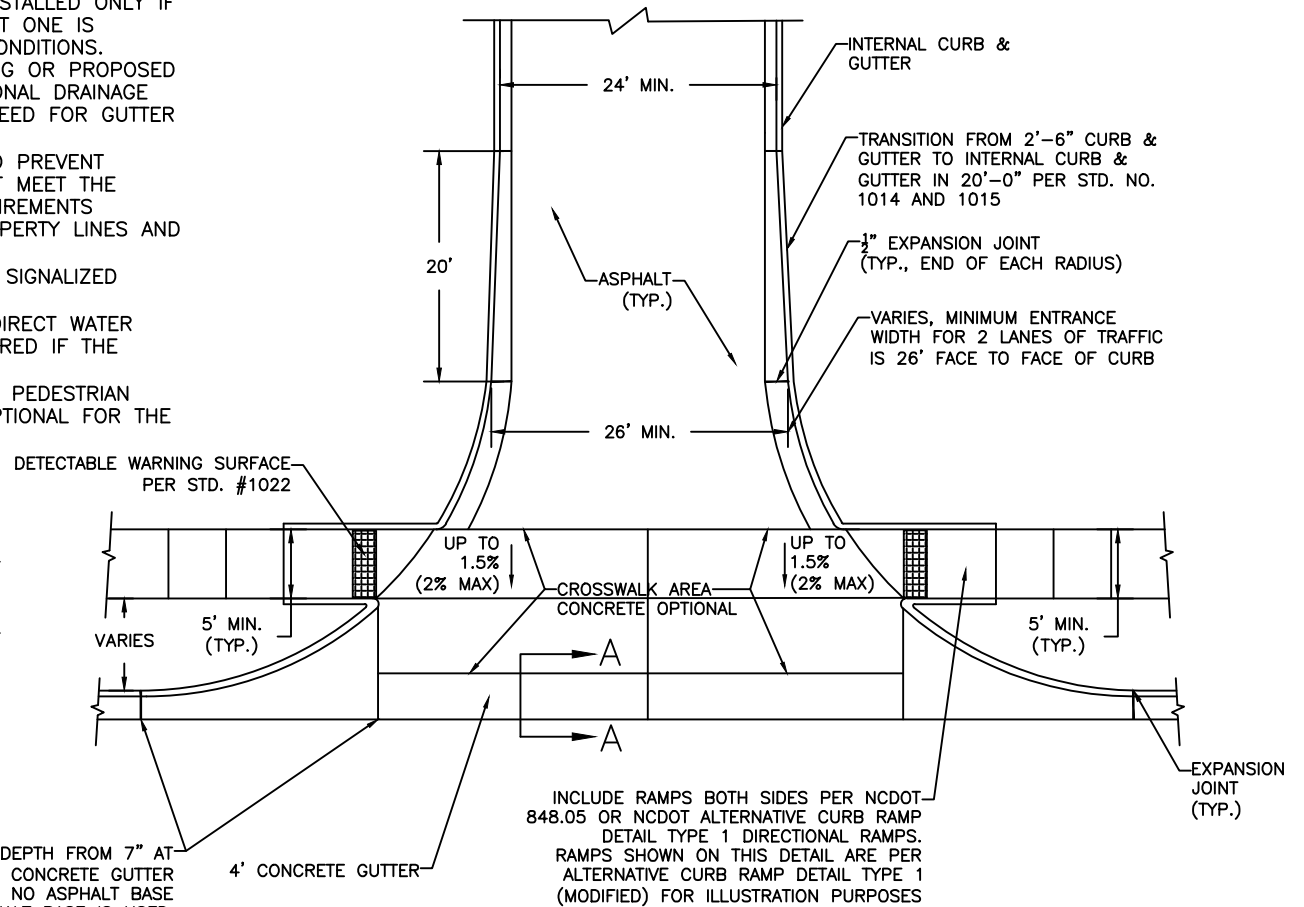
NOTES:

1. WHERE A DRIVEWAY IS APPROVED BY THE CITY OF LEXINGTON ENGINEERING STAFF THAT CONNECTS TO AN EXISTING SIGNALIZED INTERSECTION, OR AT A LOCATION WHERE A TRAFFIC SIGNAL INSTALLATION IS PROPOSED BY THE CITY OF LEXINGTON OR NCDOT BASED ON A TRAFFIC IMPACT/SIGNAL WARRANT STUDY, A FULL DEPTH ASPHALT PAVEMENT IS REQUIRED. THIS PAVEMENT DESIGN IS REQUIRED IN THE DRIVEWAY EASEMENT (100-FOOT MINIMUM) TO MAINTAIN DETECTOR LOOPS AND PAVEMENT MARKINGS. A TRAFFIC SIGNAL WILL BE INSTALLED ONLY IF CITY OF LEXINGTON ENGINEERING STAFF DETERMINES THAT ONE IS NECESSARY BASED ON A TRAFFIC STUDY OF CURRENT CONDITIONS.
2. A CONCRETE GUTTER IS TO BE USED EXCEPT AT EXISTING OR PROPOSED TRAFFIC SIGNAL LOCATIONS. AT THESE LOCATIONS ADDITIONAL DRAINAGE REQUIREMENTS WILL BE NECESSARY TO ELIMINATE THE NEED FOR GUTTER ACROSS THE DRIVEWAY CONNECTIONS.
3. THE DRIVEWAY MUST RISE 6" FROM THE GUTTER LINE TO PREVENT RUNOFF FROM ENTERING DRIVEWAY. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS
4. FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
5. TWO (2) CURB RAMPS PER CURB RETURN REQUIRED AT SIGNALIZED INTERSECTIONS.
6. FOUR (4) FOOT GUTTER AND WINGS ARE REQUIRED TO DIRECT WATER ACROSS DRIVE. GUTTER AND WINGS MAY NOT BE REQUIRED IF THE DRIVEWAY GUTTER SLOPE IS GREATER THAN 2%.
7. MAINTAIN UP TO 1.5% (MAX. 2%) CROSS-SLOPE ON THE PEDESTRIAN ACCESS ROUTE BETWEEN CURB RAMPS. CONCRETE IS OPTIONAL FOR THE CROSSWALK AREA IN THE DRIVEWAY.



SECTION A-A

* TRANSITION CONCRETE DEPTH FROM 7" AT LIP TO 10" AT 4' CONCRETE GUTTER CONSTRUCTION JOINT IF NO ASPHALT BASE INSTALLED. IF ASPHALT BASE IS USED, 7" CONCRETE DEPTH CAN BE CARRIED THROUGH THE 4' CONCRETE GUTTER.



INCLUDE RAMPS BOTH SIDES PER NCDOT 848.05 OR NCDOT ALTERNATIVE CURB RAMP DETAIL TYPE 1 DIRECTIONAL RAMPS. RAMPS SHOWN ON THIS DETAIL ARE PER ALTERNATIVE CURB RAMP DETAIL TYPE 1 (MODIFIED) FOR ILLUSTRATION PURPOSES

NOT TO SCALE

COMMERCIAL TYPE 3

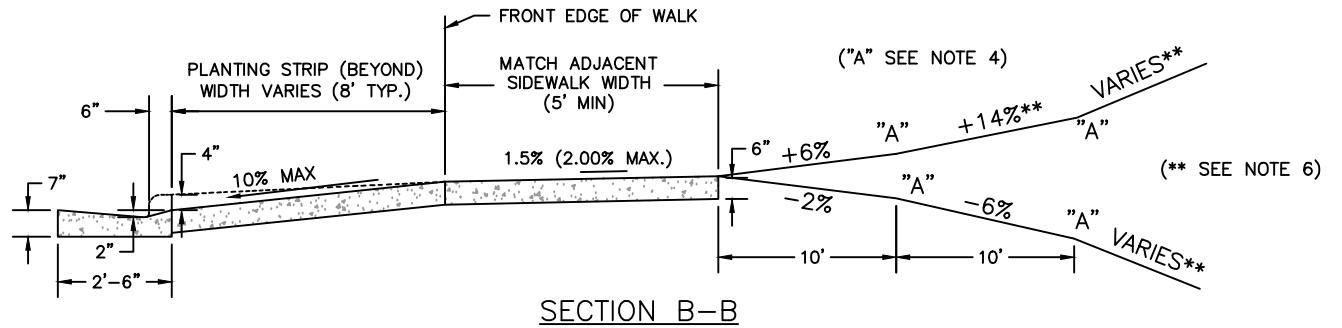
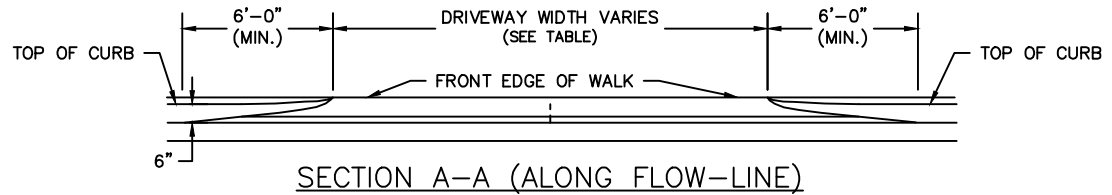
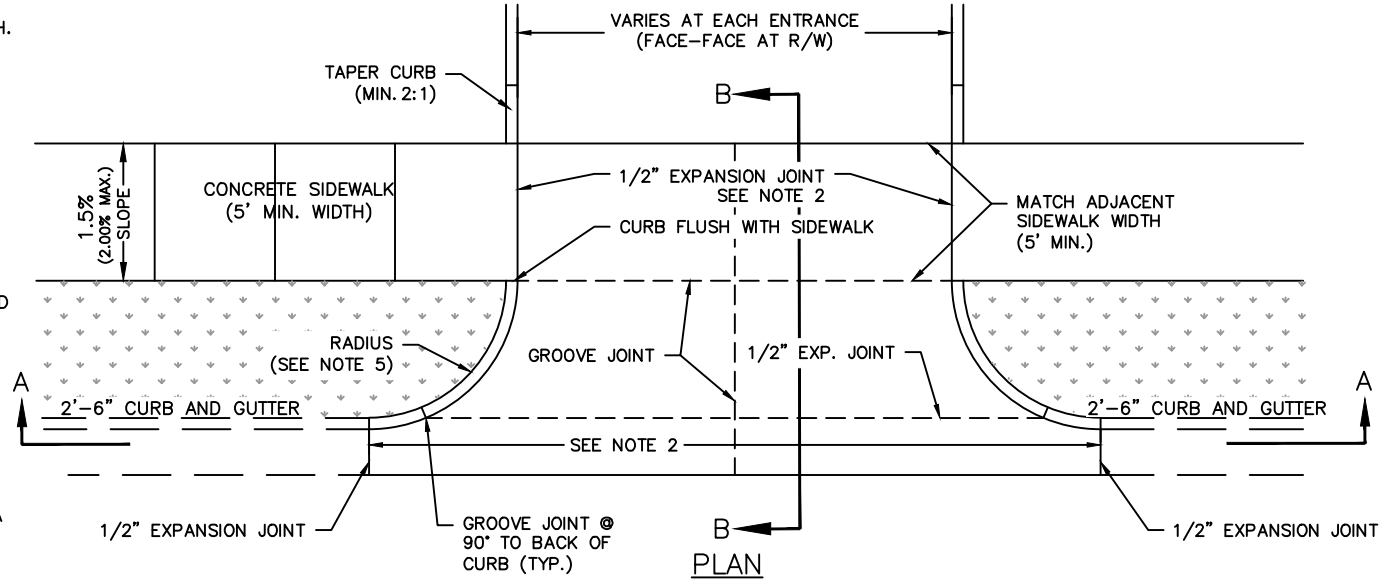
REV.	STD. NO.
1	1208



**CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS**

NOTES:

1. ALL CONCRETE TO BE 3500 P.S.I. COMPRESSIVE STRENGTH.
2. AT ALL DRIVEWAYS, SIDEWALKS TO BE REMOVED TO THE NEAREST JOINT BEYOND NEW CONSTRUCTION OR CUT WITH A SAW AND REMOVED. SAW CUT OR JOINT TO BE PERPENDICULAR TO EDGE OF EXISTING PAVEMENT. SEE STD. NO. 1012 FOR JOINT DETAIL. PAY LIMITS FOR WORK DONE UNDER CITY OF LEXINGTON CONTRACTS ARE FROM EXPANSION JOINT TO EXPANSION JOINT, FROM LIP OF CURB TO BACK OF SIDEWALK.
3. ALL DRIVEWAYS MUST MEET THE CURRENT CITY DRIVEWAY REGULATIONS AND NCDOT REQUIREMENTS FOR SPACING, SIGHT DISTANCE, AND OFFSETS FROM PROPERTY LINES AND INTERSECTIONS.
4. ALGEBRAIC DIFFERENCE IN GRADE ("A") BETWEEN SLOPES SHALL BE 8% OR LESS.
5. RADII MUST BE MINIMUM 8 FEET OR THE WIDTH OF THE PLANTING STRIP, WHICHEVER IS GREATER. RADII GREATER THAN THESE MINIMUMS MAY BE REQUIRED BY LEXINGTON ENGINEERING ON A CASE-BY-CASE BASIS. FOR RADII GREATER THAN 8 FEET, THE RADII ARE TO CONTINUE AS A BAND AT-GRADE THROUGH THE SIDEWALK.
6. PER NC IFC SECTION D103.2, FIRE APPARATUS ACCESS ROADS SHALL NOT EXCEED 10 PERCENT IN GRADE.
7. JOINT MATERIAL SHOULD BE PLACED FLUSH WITH CONCRETE.
8. THE DRIVEWAY MUST RISE 6" FROM THE GUTTER LINE TO PREVENT RUNOFF FROM ENTERING DRIVEWAY.



NOT TO SCALE

DRIVEWAY DIMENSIONS		
OPERATION/RADIUS	MINIMUM	MAXIMUM
ONE-WAY WITH 6-12 FT. RADII	20'	30'
ONE-WAY WITH 13+ FT. RADII	15'	25'
TWO-WAY WITH 6-12 FT. RADII	26'	50'
TWO-WAY WITH 13+ FT. RADII	22'	40'

**RESIDENTIAL & COMMERCIAL DRIVEWAY
WITH WIDE PLANTING STRIP
(2'-6" CURB & GUTTER)**

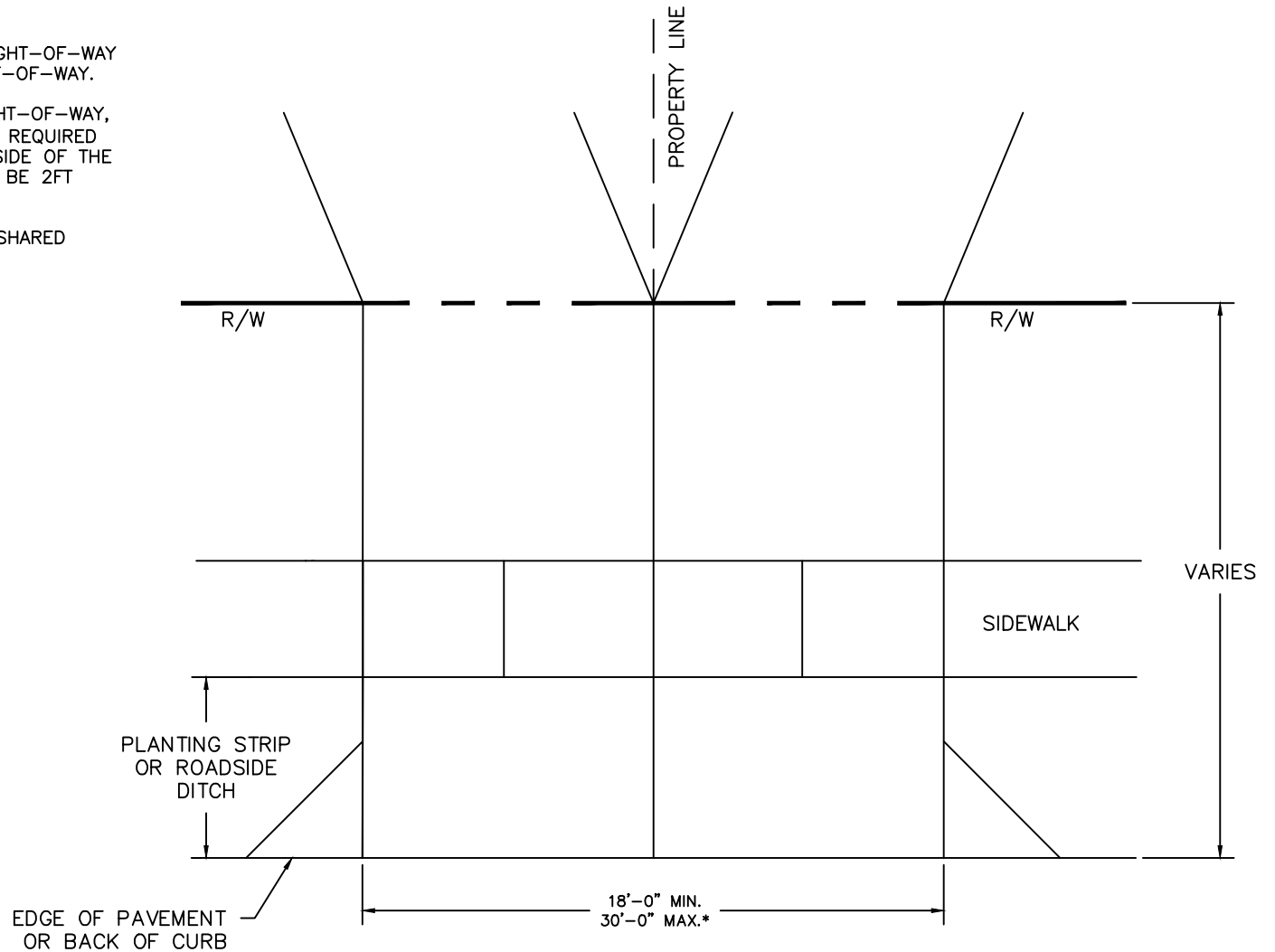
REV.	STD. NO.
1	1209



**CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS**

NOTES:

1. THIS DETAIL IS FOR MINIMUM AND MAXIMUM WIDTHS AND LAYOUT ONLY. ALL SHARED DRIVEWAYS MUST MEET THE REQUIREMENTS OF LIDS 1201 THRU 1205 SPECIFIC TO THE STREET SECTION AND/OR CURB TYPE.
2. DRIVEWAYS MUST REMAIN SHARED WITHIN THE RIGHT-OF-WAY BUT MAY SPLIT HOWEVER OUTSIDE OF THE RIGHT-OF-WAY.
3. FOR DRIVEWAYS THAT DO NOT SPLIT AT THE RIGHT-OF-WAY, A SHARED DRIVEWAY/ACCESS EASEMENT WILL BE REQUIRED FOR THE LENGTH THAT IT REMAINS SHARED OUTSIDE OF THE RIGHT-OF-WAY. WIDTH OF THE EASEMENT SHALL BE 2FT BEYOND THE DRIVEWAY ON BOTH SIDES.
4. SHARED DRIVEWAY MUST BE CENTERED ON THE SHARED PROPERTY LINE.



NOT TO SCALE

SHARED DRIVEWAY

REV.	STD. NO.
1	1210



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS