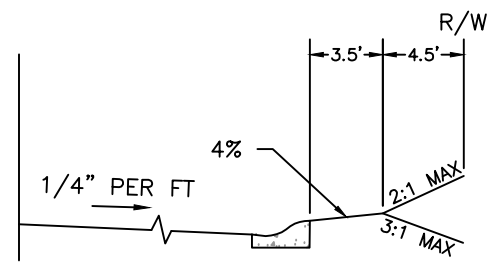


RESIDENTIAL CUL-DE-SAC



SECTION A-A

APPLICABLE WHEN NO SIDEWALK PRESENT.
PLEASE NOTE: DRIVEWAY STANDARD
MAX. SLOPES AND BREAKOVERS APPLY

NOTES:

1. ALTERNATIVE CUL-DE-SAC DESIGNS, INCLUDING ISLANDS SHALL BE SUBMITTED TO THE CITY ENGINEER FOR REVIEW AND APPROVAL.
2. THE CROWN FOR PAVEMENT SHALL BE 1/4" PER FT FROM THE CENTER OF THE CUL-DE-SAC.
3. SIDEWALK MAY BE REQUIRED TO EXTEND AROUND CUL-DE-SAC BULB WHERE PARKS OR SCHOOLS HAVE FRONTAGE TO THE END OF THE CUL-DE-SAC.

NOTE: THIS DETAIL IS NOT FOR USE ON NCDOT-MAINTAINED STREETS. REFER TO NCDOT SUBDIVISION ROADS MINIMUM CONSTRUCTION STANDARDS MANUAL.

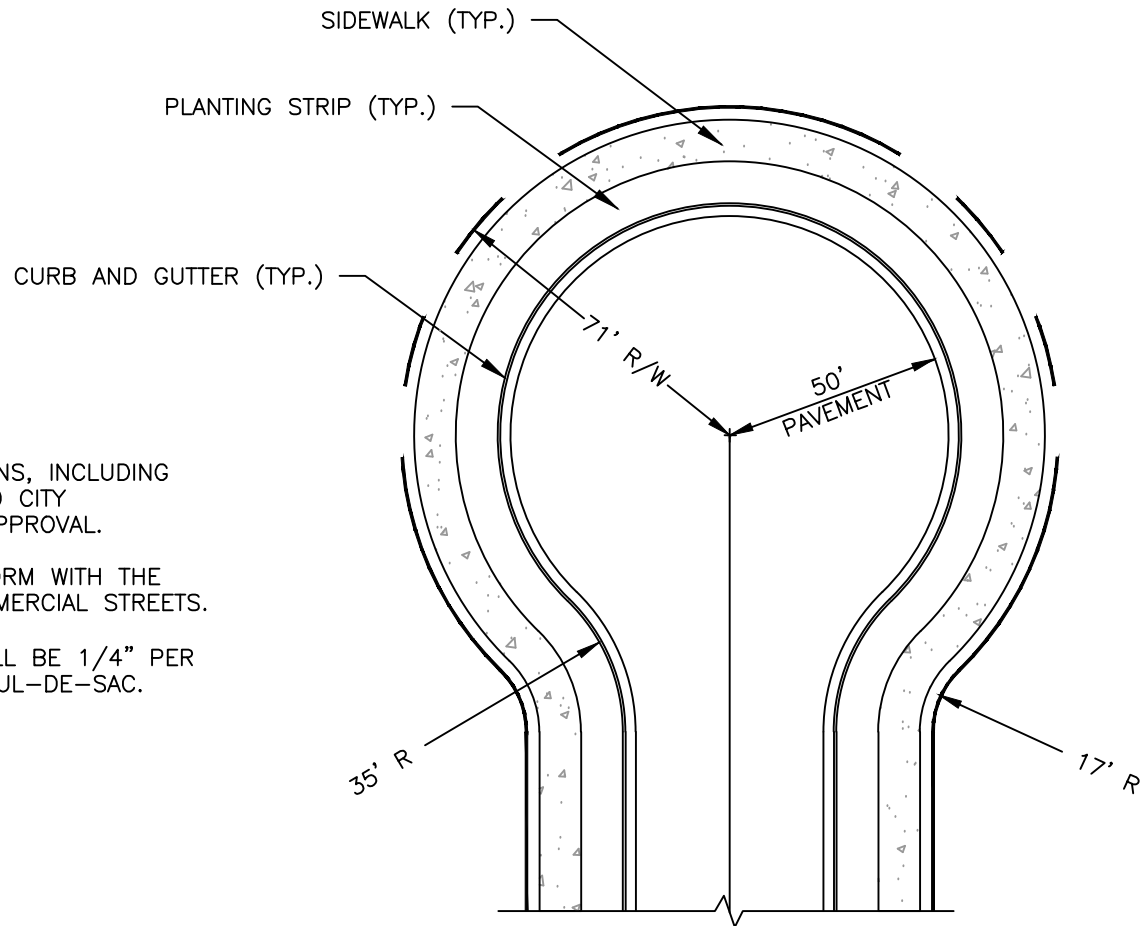
NOT TO SCALE

STANDARD CUL-DE-SAC
RESIDENTIAL

REV.	STD. NO.
1	1301



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS



NOTES:

1. ALTERNATIVE CUL-DE-SAC DESIGNS, INCLUDING ISLANDS SHALL BE SUBMITTED TO CITY ENGINEERING FOR REVIEW AND APPROVAL.
2. PAVEMENT SECTION SHALL CONFORM WITH THE DESIGN REQUIREMENTS FOR COMMERCIAL STREETS.
3. THE CROWN FOR PAVEMENT SHALL BE 1/4" PER FT FROM THE CENTER OF THE CUL-DE-SAC.

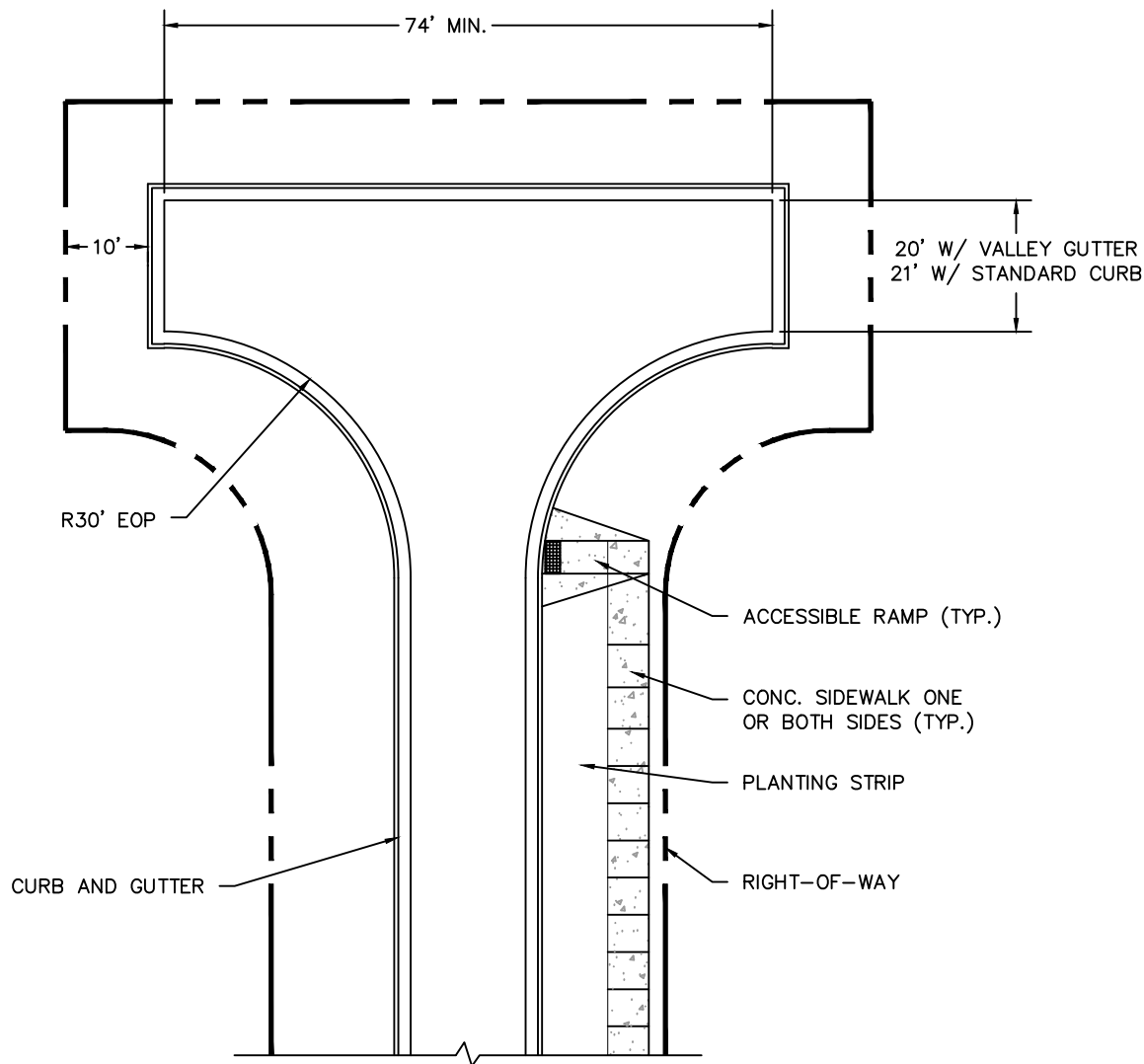
NOT TO SCALE

STANDARD CULDESAC
OFFICE/COMMERCIAL/INDUSTRIAL



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS

REV.	STD. NO.
1	1302



NOTES

1. THIS DESIGN ACCOMMODATES SINGLE-UNIT TRUCK BUT NOT A LEXINGTON FIRE DEPARTMENT LADDER TRUCK. TO DESIGN FOR A LADDER TRUCK REQUIRES A HAMMERHEAD OF 120 FEET IN LENGTH.
2. VARIATIONS ON THIS DESIGN (E.G. TURNAROUNDS IN THE STEM, ROTATION OF ENTRY POINT, ETC.) CAN BE SUBMITTED TO CITY ENGINEERING FOR REVIEW AND APPROVAL ON A CASE-BY-CASE BASIS.
3. SIDEWALK MAY BE REQUIRED TO EXTEND AROUND THE HAMMERHEAD WHERE PARKS OR SCHOOLS HAVE FRONTAGE TO THE END OF THE HAMMERHEAD
4. TEMPORARY HAMMERHEADS MAY BE GRAVEL BUT MUST FOLLOW THE SAME DIMENSIONS.

NOTE: THIS DETAIL IS NOT FOR USE ON NCDOT-MAINTAINED STREETS. REFER TO NCDOT SUBDIVISION ROADS MINIMUM CONSTRUCTION STANDARDS MANUAL.

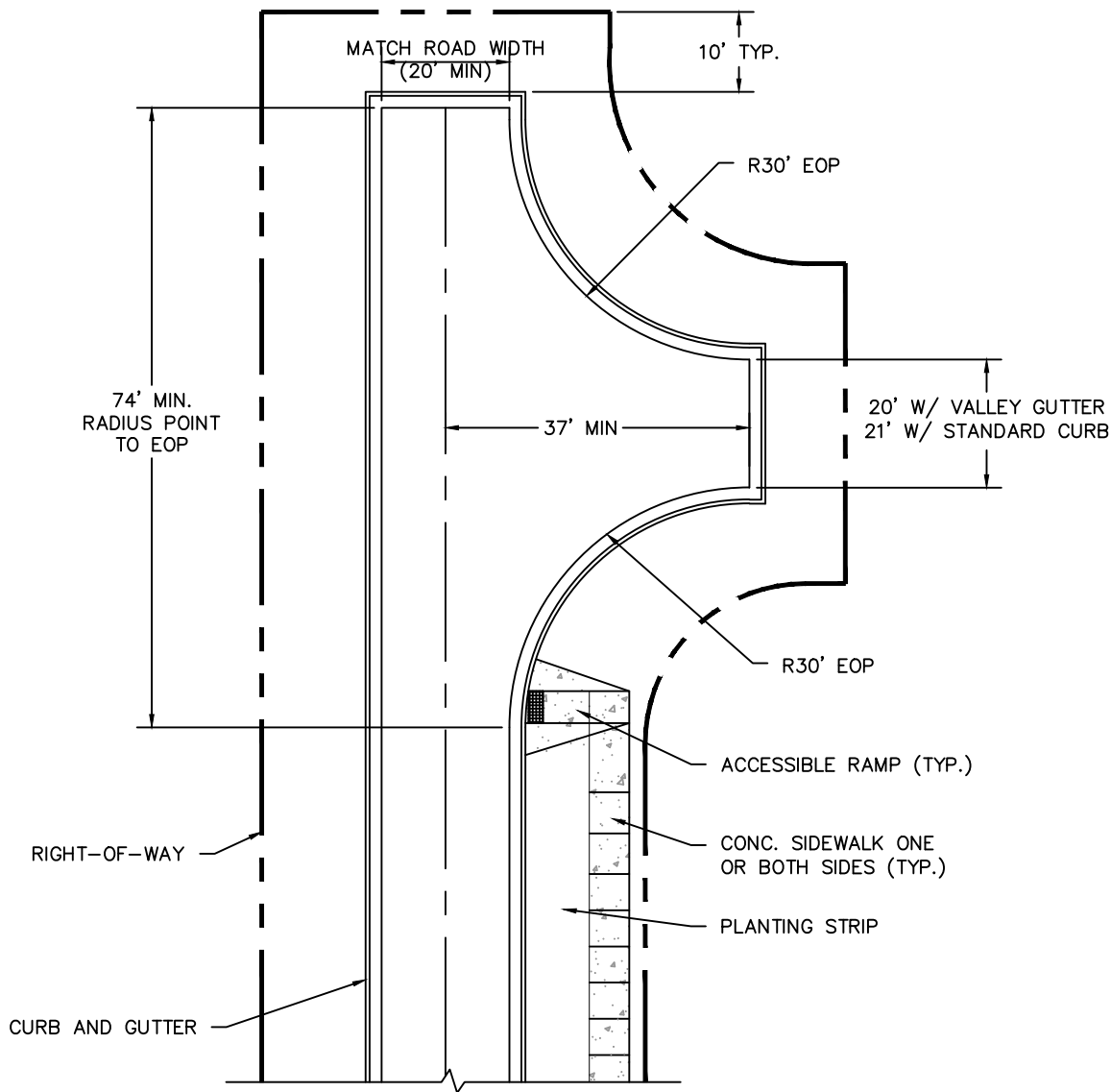
NOT TO SCALE

RESIDENTIAL HAMMERHEAD

REV.	STD. NO.
1	1303A



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS



NOTES

1. THIS DESIGN ACCOMMODATES SINGLE-UNIT TRUCK BUT NOT A LEXINGTON FIRE DEPARTMENT LADDER TRUCK. TO DESIGN FOR A LADDER TRUCK REQUIRES A HAMMERHEAD OF 120 FEET IN LENGTH.
2. VARIATIONS ON THIS DESIGN (E.G. TURNAROUNDS IN THE STEM, ROTATION OF ENTRY POINT, ETC.) CAN BE SUBMITTED TO CITY ENGINEERING FOR REVIEW AND APPROVAL ON A CASE-BY-CASE BASIS.
3. SIDEWALK MAY BE REQUIRED TO EXTEND AROUND THE HAMMERHEAD WHERE PARKS OR SCHOOLS HAVE FRONTAGE TO THE END OF THE HAMMERHEAD
4. TEMPORARY HAMMERHEADS MAY BE GRAVEL BUT MUST FOLLOW THE SAME DIMENSIONS.

NOTE: THIS DETAIL IS NOT FOR USE ON NCDOT-MAINTAINED STREETS. REFER TO NCDOT SUBDIVISION ROADS MINIMUM CONSTRUCTION STANDARDS MANUAL.

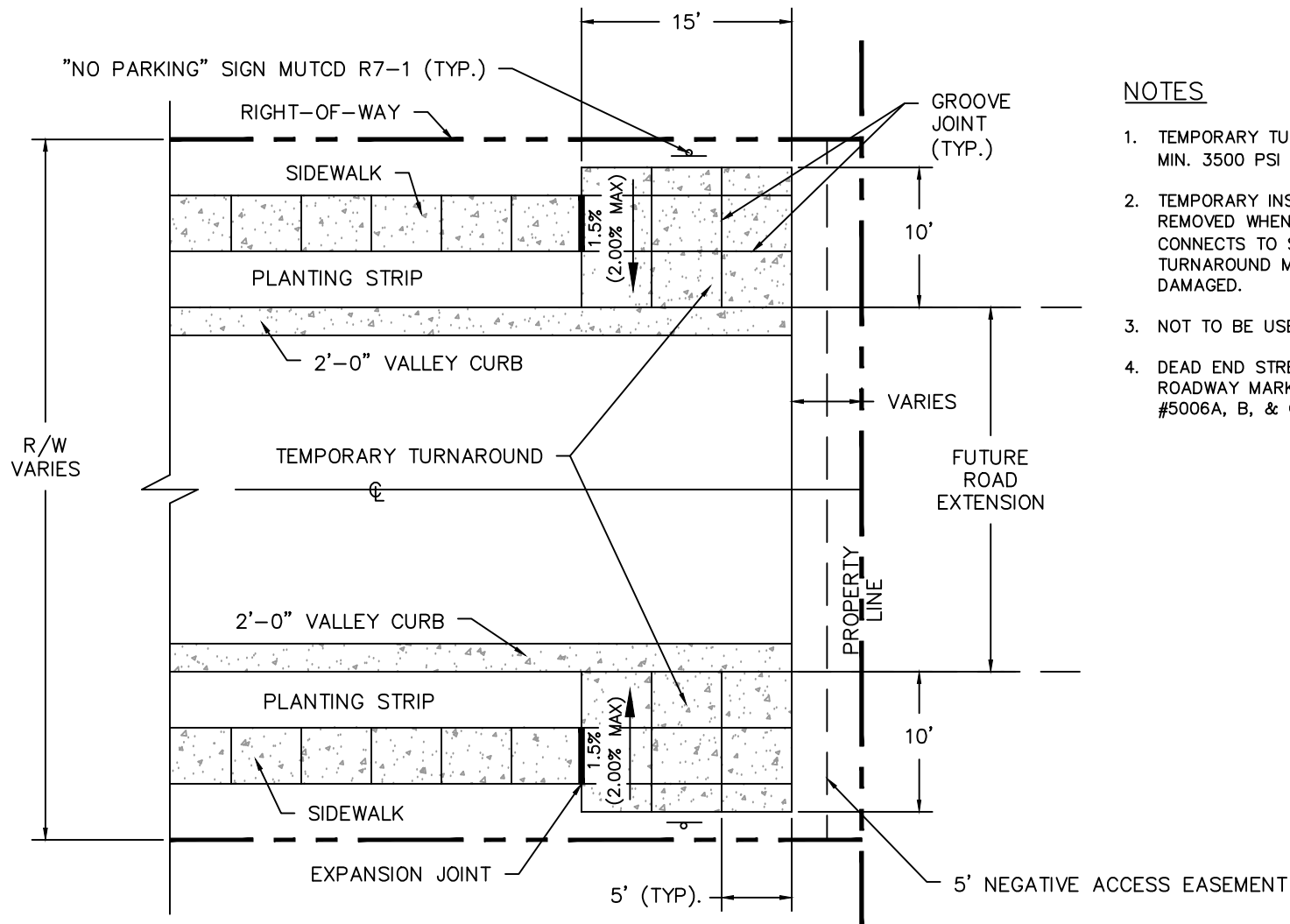
NOT TO SCALE

RESIDENTIAL WYE-HAMMERHEAD

REV.	STD. NO.
1	1303B



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS



NOTES

1. TEMPORARY TURNAROUND MATERIAL SHALL BE MIN. 3500 PSI CONCRETE, 6" THICK.
2. TEMPORARY INSTALLATION ONLY – TO BE REMOVED WHEN FUTURE DEVELOPMENT CONNECTS TO STREET. "SIDEWALK" PORTION OF TURNAROUND MAY BE LEFT IN PLACE IF NOT DAMAGED.
3. NOT TO BE USED AS A PRIVATE DRIVEWAY.
4. DEAD END STREET BARRICADE AND END OF ROADWAY MARKER PER LIDS #5005A&B AND #5006A, B, & C ARE REQUIRED.

NOT TO SCALE

TEMPORARY TURNAROUND
LOCAL RESIDENTIAL STREET

REV.	STD. NO.
1	1304



CITY OF LEXINGTON
INFRASTRUCTURE
DEVELOPMENT STANDARDS